

APPLICATION FOR A DEVELOPMENT PERMIT

MATERIAL CHANGE OF USE
MULTIPLE DWELLING (AFFORDABLE HOUSING –
48 DWELLING UNITS)

on behalf of
Urbex Pty Ltd

at
2 Treeline Drive, Rasmussen

on
Lot 904 on SP344615

Somers & Hervey
ESTATE
urbex





Brazier Motti have prepared this report for the sole purposes of Urbex for the specific purpose of a Development Application seeking a Development Permit for Material Change of Use (Multiple Dwelling) at 2 Treeline Drive, Rasmussen.

In preparing this report we have assumed that all information and documents provided to us by others, such as the client, other consultants acting on the client's behalf or government agencies, to be complete, accurate and current.

Signed on behalf of Brazier Motti Pty Ltd

ANNE ZAREH

Senior Town Planner

Brazier Motti



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1.0 INTRODUCTION

This town planning report has been prepared on behalf of the Applicant, Urbex Pty Ltd, in support of a development application seeking a Development Permit for a Material Change of Use for Multiple Dwelling (Affordable Housing – 48 Dwellings) at 2 Treeline Drive, Rasmussen, being formally described as Lot 904 on SP344615.

The proposal is for 48 x multiple dwelling units within the Somers & Hervey Estate. It will provide a much-needed affordable housing product at a strategic location.

To assist in Council's determination of this development application, this planning report covers the following matters:

Section 2:- Subject land description.

Section 3:- Subject site description.

Section 4:- A detailed description of the development proposal.

Section 5:- A review of relevant legislative requirements.

Section 6:- An assessment of the proposal against the relevant code provisions of the Townsville City Plan 2014.

Section 7:- Conclusion.

The development application is made in accordance with section 51 of the Planning Act 2016 and contains the mandatory supporting information specified in the applicable DA forms, included in **Appendix A**. The application is subject to impact assessment and therefore public notification will be required.

For the purpose of this report, the subject land refers to Management Lot 904 on SP344615 which includes future Precincts 2, 3 and 4, whereas the subject site refers to the proposed englobo Lot (Stage 12A) in Precinct 4.



2.0 THE SUBJECT LAND

The subject land is formally described as Lot 904 on SP344615 and is located at 2 Treeline Drive in the Upper Ross suburb of Rasmussen with direct access to established education and community facilities as well as local services and convenience shopping. Proximity to the Ross River corridor provides broader access to regional open space facilities and local bus routes run direct to the Thuringowa retail and commercial precinct.

Figure 1 below shows the extent of the subject land. It encompasses a total area of 29ha.

Figure 1: Aerial image of the Subject site



Source: Queensland Globe

The Certificate of Title confirming ownership of the subject site by Urbex 118 Pty Ltd is included in **Appendix B**. A copy of the survey plan is also included in **Appendix B**.



With respect to planning scheme designations, the subject land is included within the Low Density Residential zone (refer Figure 2).

Figure 2: Zoning of the Subject Land



Source: Extract of the Townsville City Plan 2014 – Zoning Map



3.0 THE SUBJECT SITE

The subject site forms part of Lot 904 on SP344615 and is located in the north-eastern section of Lot 904. Further, the subject site is located within Precinct 4 of the Somers & Hervey Estate. *Figure 3* below shows the extent of the subject site which encompasses an area of approximately 1.714ha.

Figure 3: Aerial image of the Subject site – black outlines



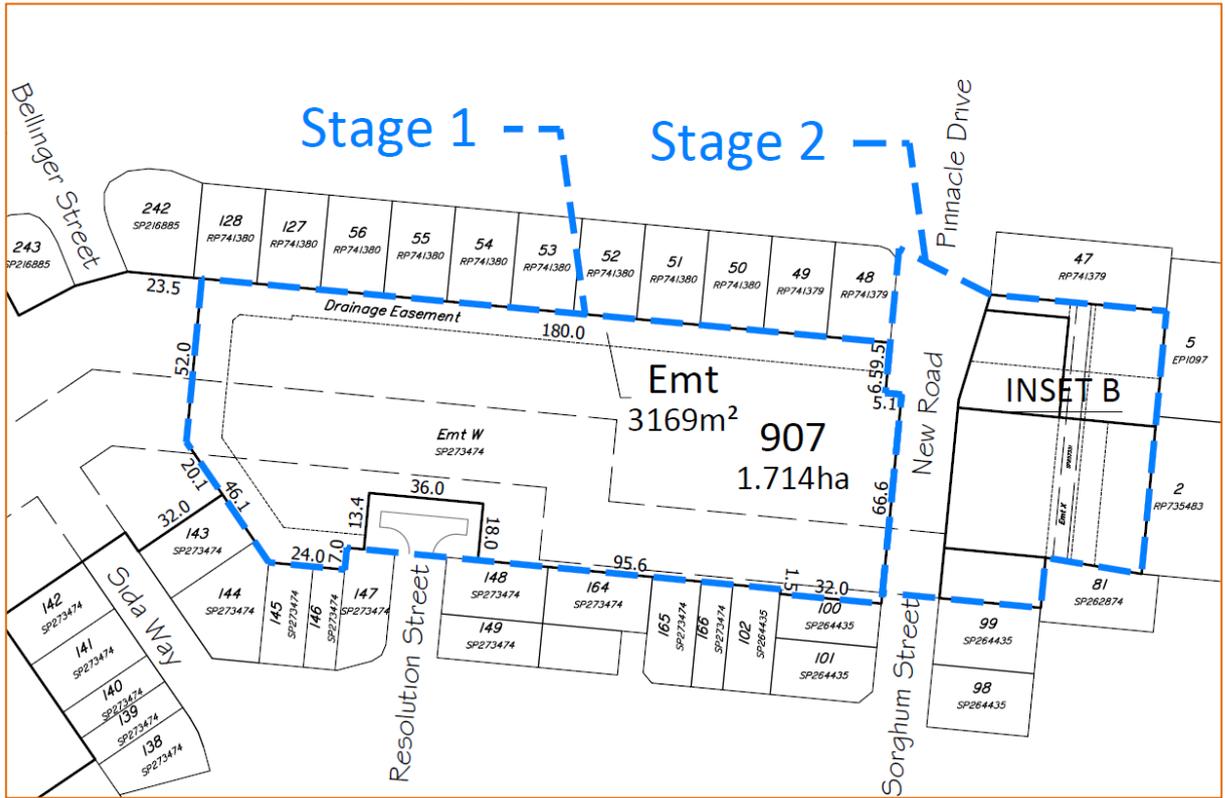
Source: Queensland Globe

Current Application over the subject land

RAL26/0003 which is currently being assessed by Council seeks the creation of an englobo allotment (Stage 1 in Stage 12A). The englobo lot is earmarked as the future affordable housing site, subject to this application. A reconfiguring a lot plan has been submitted as part of RAL26/0003 to show the proposed englobo lot, refer to *Figure 4*.



Figure 4: RAL Proposal Plan under assessment RAL26/0003



Source: Brazier Motti



4.0 THE PROPOSAL

The applicant proposes to deliver an affordable housing product within the existing residential Somers & Hervey Estate as identified on *Figure 5* below. The development will include the establishment of 48 separate dwellings with carport and private open space area. In total, the proposed development will incorporate 41 x one-bedroom dwellings and 7 x two-bedroom dwellings. Further, the development proposal is illustrated in detail on the attached development plans which have been prepared by Thinktank Architects, and are included in **Appendix C**.

Figure 5: Site Layout Plan



Source: Thinktank Architects

The proposed use is defined as follows under the Townsville City Plan 2014:-

Multiple Dwelling - Premises containing three or more dwellings for separate households.

The table below shows an overview of the dwelling structure and associated GFA for each building.

Table 1- Overview- Building GFA

| Buildings | |
|------------------------------|--|
| 41 x 1 Bedroom Dwellings | 2097.27m ² (51.15m ² per dwelling) |
| 7 x 2 Bedroom Dwellings | 527.72m ² (75.4m ² per dwelling) |
| Total Residential GFA | 2625m² |



The design of the development provides direct streetscape interaction with the internal road network and connection into the wider external road network. Resident access is provided from Pinnacle Drive (connecting missing link between Pinnacle Drive and Sorghum Street) and via private internal road with driveway access to each dwelling. An internal footpath runs along dwelling units 12-22 and 45-48 which links into the external footpath network in Pinnacle Drive/Sorghum Street.

The dwelling layouts have been carefully considered, with private spaces situated behind screening, and living spaces have direct access to patio areas. Each unit has a carport and associated tandem visitor car park. Refer to floor plans in **Appendix C**.

Four different colour schemes and materials have been applied to add visual variety. *Figure 6* shows the internal streetscape elevations.

Figure 6: Internal Streetscape plans







Source: Thinktank Architects

The use of these materials creates a synergy between the dwelling types. *Figures 7-10* below show the proposed elevations and corresponding views and materials of each dwelling type.

Figure 7: 1 Bedroom – Type 1 Elevations and Views



Source: Thinktank Architects



Figure 8: 1 Bedroom – Type 2 Elevations and Views



Source: Thinktank Architects

Figure 9: 2 Bedroom – Type 1 Elevations and Views



Source: Thinktank Architects



Figure 10: 2 Bedroom – Type 2 Elevations and Views



Source: Thinktank Architects

Proposed Tenure- Building Format Plan

The future Community Titling of the units will be by way of a building format plan arrangement. The internal road network, visitor carparks, bin store, letter boxes will form part of the common property. Each dwelling will be a separate lot with an associated exclusive use area (private open space).

Community open space, bin enclosure and letter boxes will be located at the entry (future common property) of the subject site with access via internal and external footpath connection.

Further, artist impressions have been included in the architectural design package showing perspective views within the development site.



5.0 RELEVANT LEGISLATION

5.1 COMMONWEALTH LEGISLATION

The application is not subject to assessment against Commonwealth legislation. It is not anticipated that development of this land will trigger assessment against the Environmental Protection and Biodiversity Conservation Act 1999 (EPBC), as it is not anticipated that the development will significantly impact upon a matter of national environmental significance.

5.2 THE PLANNING ACT 2016

The Planning Act 2016 provides the framework for coordinating local, regional and state planning. Given the nature of the development, the application requires assessment against this legislation.

5.3 STATE ASSESSMENT AND REFERRALS

The Development Assessment Rules (DA Rules) incorporate a referral process, established through the *Planning Regulation 2017*, enabling relevant State agencies to have input in the assessment process. Schedule 10 of the *Planning Regulation 2017* outlines the triggers for the referral of the development application to other agencies. The referral agencies for the development application are summarised in *Table 2*.

Table 2- State referral triggers

| ISSUE | STATUTORY TRIGGER |
|----------------------------|---|
| Electricity Infrastructure | <p><u>Schedule 10, Part 9, Division 2, Table 2, Item 1</u></p> <p>Development application for a material change of use that is assessable development under a local categorising instrument and does not relate to reconfiguring a lot, if—</p> <p>(a) all or part of the premises are within 100m of a substation site; or</p> <p>(b) both of the following apply—</p> <p>(i) all or part of the premises are subject to an easement for the benefit of a distribution entity, or transmission entity, under the Electricity Act;</p> <p>(ii) the easement is for a transmission grid or supply network</p> |

5.4 STATE PLANNING POLICY

For the purposes of this development, we consider that separate assessment of the proposal against the provisions of the SPP is not required given that all relevant matters will be dealt with under the provisions of the planning scheme.

5.5 ASSESSMENT MANAGER AND PLANNING SCHEME

Townsville City Council is nominated as the assessment manager for the application. The applicable planning scheme is the City Plan 2014.



5.6 NORTH QUEENSLAND REGIONAL PLAN

The subject site is located within an urban area identified in the NQ Regional Plan, refer *Figure 11* below. The development is consistent with the outcomes of the NQ Regional Plan as it further strengthens the residential sector to support Townsville's growing community at a convenient location, with strong connections to public transport and principal roads.

Figure 11: NQ Regional Plan- Urban Area



Source: DAMS Mapping

5.7 PUBLIC NOTIFICATION

The proposed development on the subject site requires public notification under the provisions of the Planning Act 2016.



6.0 THE PLANNING FRAMEWORK

6.1 THE TOWNSVILLE CITY COUNCIL PLANNING SCHEME – CITY PLAN 2014

The planning scheme seeks to achieve outcomes through the identification of a number of overall outcomes, performance outcomes and acceptable solutions. Land identified within the planning scheme is divided into a number of zones. Zones are further identified within individual precincts. The planning scheme further identifies numerous overlay codes.

6.2 PLANNING SCHEME DESIGNATIONS

In accordance with the Townsville City Plan 2014, the site is subject to the designations listed in *Table 3*. These designations will assist in determining which Tables of Assessment are applicable to the proposed development on the subject site and therefore assist in the determination of the Category of assessment and the codes applicable to the proposed development.

Table 3- Planning Scheme Designation

| Type of Designation | Designation |
|---|--|
| Zone | Low Density Residential Zone |
| Precinct | Not Applicable |
| Local Area | Not Applicable |
| Airport Environs Overlay | Identified within the Operational Airspace Overlay more than 90m above ground level. |
| Bushfire Hazard Overlay | Not applicable |
| Coastal Protection Overlay | Not Applicable |
| Cultural Heritage Overlay | Not Applicable |
| Natural Assets (Extractive Resources Overlay) | Not applicable |
| Flood Hazard Overlay | Identified within the low, medium and high hazard area |
| Landslide Hazard Overlay | Not applicable |
| Natural Assets (Environmental Importance) Overlay | Not applicable |
| Natural Assets (Water Resource Catchment Overlay) | Not applicable |

6.3 LEVEL OF ASSESSMENT, ASSESSABLE BENCHMARKS AND APPLICABLE CODES

The subject site is designated within the *Low Density Residential Zone*, *Airport Environs Overlay* and *Flood Hazard Overlay*. The relevant table of assessment within the Townsville City Plan (2014) (Table 5.5.1) identifies the Material Change of Use for Multiple Dwelling as Impact Assessable Development.



Furthermore, the Assessment Tables identify that an application for impact assessable Material Change of Use requires assessment against the planning scheme and hence the following Codes:

- Strategic Framework;
- Low Density Residential Zone Code;
- Healthy Waters Code;
- Landscape Code;
- Transport Impact, Access and Parking Code;
- Works Code;
- Airport Environs Overlay Code; and
- Flood Hazard Overlay Code.

A complete assessment of the proposal against the relevant provisions of the applicable Codes is discussed below:

6.4 STRATEGIC FRAMEWORK

The Framework relevantly contemplates four themes that represent the policy intent of the Scheme. The themes are:

1. Shaping Townsville;
2. Strong, connected community;
3. Environmentally sustainable future; and
4. Sustainable economic growth.

The themes, on balance, seek to create a vibrant and liveable City that is efficiently served and respectful of its built and natural environment.

The proposed development achieves this balance, located at a convenient and accessible location within a prominent area that is earmarked for infill housing. The project maximises utility of established infrastructure improving efficiencies and allows the activation of a strategically located site.

The proposed development is in direct response to the housing crisis. As identified in Council's Local Housing Action Plan over 4000 Townsville residents are in need of affordable housing, with 2,545 experiencing rental stress and 1570 experiencing homelessness or are marginally housed. Continual housing market pressure will continue to exacerbate the demand for affordable housing in the Townsville LGA.

It has been concluded that the proposed development advances the intent of the strategic frameworks as it:-

- Provides much needed medium-density housing at a strategic location within an existing residential estate;
- Provides much needed affordable housing in an infill area;
- Concentrates land use at a key location and offers affordable residential housing opportunities; and
- Supports the efficient provision of infrastructure.



In summary, the development will contribute to the diversity and choice of housing that is available across the City and will assist in supply at a time of recognised housing pressure. There is a demonstrated demand for affordable housing that is not otherwise currently available in the Townsville market.

Provision of this type of housing product will open supply to the market and it provides an alternate product to assist in responding to Council's Townsville Affordable Housing Action Plan which includes in its purpose to support options that seek to improve access to housing, retain and attract key workers as well as activate the city.

The development respects its setting and provides a coherent transition from surrounding traditional residential densities to a low-medium density.

The development is capable of accommodating required services without burden and it offers an improved density of development that will assist in optimising the efficient use of established infrastructure, leveraging investment already made by the State and Council in the locality.

6.5 ZONE CODE PROVISIONS

6.5.1 Low Density Residential Zone Code

In accordance with the Townsville City Plan 2014 the subject site is contained within the Low Density Residential zone, where Material Change of Use for Multiple Dwelling is considered Impact Assessable.

The purpose of the Low density residential zone code is to provide for predominately dwelling houses.

The particular purpose of the code is:

- a) *primarily accommodate dwelling houses and dual occupancy dwellings;*
- b) *provide for housing choice and affordability by allowing for a range of lot sizes;*
- c) *maintain the low-rise and lower density character of Townsville's suburbs;*
- d) *maintain a high level of residential amenity; and*
- e) *achieve accessible, well-serviced and well-designed communities.*

The purpose of the zone will be achieved through the following overall outcomes:

- a) *built form is of a house compatible scale and consistent with the local streetscape character;*
- b) *reconfiguration facilitates a range of lot sizes to accommodate dwelling houses and dual occupancy dwellings, typically on lots 400m² or greater (unless otherwise intended in a precinct);*
- c) *development maintains a high level of residential amenity on the site and in the neighbourhood;*
- d) *residential development is protected from the impacts of any nearby industrial activities, transport corridors, infrastructure, installations and major facilities such as the port, airport and Department of Defence landholdings;*
- e) *the design of development promotes accessibility by walking, cycling and public transport;*
- f) *the design of development is responsive to the city's climate and promotes sustainable practices including energy efficiency and water conservation;*
- g) *low-rise multiple dwellings, residential care or retirement facilities may be acceptable within this zone where provided in locations that are within a convenient walkable distance to centres, public transport and community activities, and where of a scale that is consistent with the local character;*



- h) non-residential uses only occur within the zone where they primarily support the day-to-day needs of the immediate residential community, do not unreasonably detract from the residential amenity of the area and are not better located in nearby centre zones;
- i) development minimises impacts on remaining areas of ecological significance within the zone; and
- j) the natural bushland setting and village character of the northern beaches and Magnetic Island townships are maintained. Tourism accommodation in these communities is of a house compatible scale.

Response

It is considered that the proposed Multiple Dwellings are strongly consistent with the purpose and strategic outcomes of the Low Density Zone. The proposal is to establish multiple dwelling units to provide for housing choice and affordability. The proposal has focused strongly of providing residential amenity for the residents of the proposed units, along with maintaining amenity for surrounding residential lots. The development is consistent with the zone intent as it provides low-rise multiple dwellings, where provided in locations that are within a convenient walkable distance to public transport and community activities, and where of a scale that is consistent with the local character.

The proposed development is consistent with the overall outcomes of this code, specifically noting that:-

- The proposed use is anticipated within the zone and locality and provides an easily accessible and contemporary buildings;
- The new development meets the needs of community, and in particular the need of residential affordable housing;
- The new built integrates well into the existing amenity and connects to the external road network.

A detailed assessment against relevant benchmarks is provided below:-

Table 4- Low Density Residential Code Assessment

| Performance outcomes | Acceptable outcomes | Assessment |
|---|--|--|
| Amenity | | |
| <p>PO10 Development minimises impacts on surrounding land and provides for an appropriate level of amenity within the site, having regard to:</p> <ol style="list-style-type: none"> 1. noise; 2. hours of operation; 3. traffic; 4. visual impact; 5. odour and emissions; 6. lighting; 7. access to sunlight; 8. privacy; and 9. outlook. | <p>No acceptable outcome is nominated.</p> | <p>Complies The proposed development is a logical extension to the existing Somers and Hervey Estate located within the Low Density Residential Zone of Rasmussen. Specifically, this section of proposed Precinct 4 will provide infill development in form of affordable housing providing a higher density product to address the housing shortage.</p> <p>The proposed infill development is located in an ideal location and is close to community facilities as well as local services and convenience shopping. The proposed development complies with the relevant requirements to enable an appropriate level of amenity to be maintained upon neighbouring land.</p> <p>Traffic has been considered as part of this application and can be managed appropriately (refer to the SLR Traffic Impact Assessment in Appendix F). Further, noise has also been considered by SLR, refer to Appendix G.</p> |



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| <p>PO11 Landscaping is provided to enhance the appearance of the development, screen unsightly components, create an attractive on-site environment and provide shading.</p> | <p>No acceptable outcome is nominated.</p> | <p>Complies The overall masterplan anticipates approximately 10.15ha of the project area will be delivered as open space with an additional 7.19ha of Council open space to be embellished to meet design standards and community expectations. Within the Somers & Hervey project, two local parks have already been delivered, being Jeff Caddies Park and Birdwing Park. A smaller local park (Alfresco Park) was also constructed along Basil Lane to cater for a cluster of small lots in one of the last stages delivered by DHA.</p> <p>The proposed development will complement the local streetscape image in accordance with Council policies (street trees, etc.). A detailed landscape concept plan has been prepared by RPS (Refer to Appendix D) showing the landscape intent for the proposed development. It shows how the development will link into the existing open space network.</p> <p>It is expected that landscaping can be conditioned as part of any development permit to form part of subsequent operational works applications.</p> |
| <p>Crime prevention through environmental design</p> | | |
| <p>PO13 Development facilitates the security of people and property having regard to:</p> <ol style="list-style-type: none"> 1. opportunities for casual surveillance and sight lines; 2. exterior building design that promotes safety; 3. adequate lighting; 4. appropriate signage and wayfinding; 5. minimisation of entrapment locations; and 6. building entrances, loading and storage areas that are well lit and lockable after hours. | <p>No acceptable outcome is nominated.</p> | <p>Complies The development design allows for passive surveillance to occur within the subject site. Appropriate lighting and signage will be provided throughout the development.</p> <p>Clear sightlines and open layouts in communal areas will be designed to minimise visual obstructions, enhancing natural surveillance and contributing to a secure atmosphere throughout the community.</p> <p>Well-lit pathways and pocket parks will ensure visibility during evening hours, creating a safer environment for residents and encouraging the use of outdoor spaces after dark. Refer to Appendix D.</p> |
| <p>Parking and servicing</p> | | |
| <p>PO16 Parking facilities are located to be concealed from public view to ensure an attractive streetscape.</p> | <p>AO16 Vehicle parking structures are located:</p> <ol style="list-style-type: none"> 1. behind the building setback; or 2. behind the building; or 3. at basement level. | <p>Complies Car parking will be provided in form of a carport included in the dwelling design and tandem space. A short term servicing bay is provided at the entry of the subject site, near the bin enclosure.</p> |



| | | |
|--|--|--|
| <p>PO17 Waste disposal and servicing areas are screened from public view and do not have adverse amenity impacts on adjoining properties.</p> | <p>No acceptable outcome is nominated.</p> | <p>Complies The site will be serviced with the use of bulk bins. Bulk bin storage is conveniently located at the front entry of the site. The bulk bins will be stored in an enclosure that restricts visibility of the bins from the street frontage. A hose cock and hose will be fitted in close proximity to the bulk bin storage enclosure.</p>  <p>Waste recovery vehicles servicing the bulk bins are able to enter and exit the site in a forward gear. Refer to Appendix F. Waste collection will be via Council's weekly collection service and residents will be able to make their bins available for service on the street frontage at the relevant times. This is not dissimilar to the servicing arrangements currently being used elsewhere in Townsville.</p> |
|--|--|--|

Location

| | | |
|--|--|--|
| <p>PO21 Multiple dwelling, residential care facility or retirement facility development occur only in locations where:</p> <ol style="list-style-type: none"> 1. they can be designed to be compatible with local character and low density scale of development; 2. having convenient walkable access to the city's network of centres; 3. having convenient walkable access to public transport; and 4. having convenient walkable access to recreational facilities. | <p>AO21 The premises:</p> <ol style="list-style-type: none"> 1. is located within 400m of a centre zone or public transport stop on a major road; 2. has a minimum site area of 1,000m² and 20m frontage where for a multiple dwelling; 3. has a minimum site area of 3,000m² and 20m frontage where for a residential care facility or retirement facility; and 4. is located within 400m of a park. | <p>Complies The proposed development integrates well into the existing Somer's and Hervey Estate and consists of a density that is not inconsistent with the surrounding area. The proposal is to provide affordable housing in the form of townhouses. This represents a gentle density that is in keeping with the existing settlement structure. Proposed lots will have areas between approximately 178m² and 227m².</p> <p>The site has an area well above the minimum requirement (1000m²) and is located within 400m of parkland. Further, the development will integrate well into the existing pedestrian network enabling walkable access to public transport and recreational facilities.</p> |
|--|--|--|

| | | |
|--|---|---|
| <p>PO22 Buildings are low-rise and of a house compatible scale.</p> | <p>AO22 Building design achieves:</p> <ol style="list-style-type: none"> 1. a site cover that does not exceed 65% of site area; 2. a building height that does not exceed 2 storeys and 8.5m if it is within 10m of a site boundary, and 3 storeys if it is 10m or more from a site boundary; and 3. the maximum length of any wall is 12m. | <p>Complies The proposed site cover is well below 65% and proposed buildings will be single storey. Wall length will not exceed 12m.</p> |
|--|---|---|



| | | |
|---|--|--|
| <p>PO23 Building setbacks and landscaping:</p> <ol style="list-style-type: none"> 1. create an attractive, consistent and cohesive streetscape; 2. maintain appropriate levels of light and solar penetration, air circulation, privacy and amenity for existing and future buildings; and 3. do not prejudice the development or amenity of adjoining sites. | <p>AO23.1 Buildings are set back from street frontages:</p> <ol style="list-style-type: none"> 1. within 20% of the average front setback of adjoining buildings; or 2. where there are no adjoining buildings, 3m. <p>AO23.2 A landscape area with a minimum dimension of 1m is provided along the full frontage of any road frontage (excluding crossover and pedestrian access only). Figure 6.6 – Appropriate front and rear setbacks illustrates.</p> <p>AO23.3 The side boundary setback is a minimum of:</p> <ol style="list-style-type: none"> 1. 1.5m for a wall up to 4.5m high; 2. 2m for a wall up to 7.5m high; and 3. 2.5m for any part of a wall over 7.5m high. <p>AO23.4 The rear boundary setback is a minimum of 6m. Figure 6.6 - Appropriate front and rear setbacks illustrates.</p> | <p>Complies Building setbacks allow for inclusion of landscaping and streetscaping. For full landscaping detail refer to Appendix D.</p>  <p>The proposed development provides appropriate setbacks which complement the local setting and create an attractive and interactive streetscape that will integrate into Saltbush Boulevard. The proposed setbacks are considerate of the adjoining development context. The setbacks are shown on the concept design plans. Refer to Appendix C.</p> |
| <p>PO24 Built to boundary walls do not impact on the amenity or privacy of adjoining premises.</p> | <p>AO24 Built to boundary walls:</p> <ol style="list-style-type: none"> 1. are for non-habitable rooms or spaces only; 2. are not located within the front or rear setback; 3. are not located within 1.5m of a habitable room or house on an adjoining lot; 4. have a maximum height of 3m; and 5. have a maximum length of 9m. | <p>Not applicable</p> |
| <p>PO25 Buildings are designed to achieve good solar access by:</p> <ol style="list-style-type: none"> 1. minimising the extent of shadows on usable private open space or public spaces; and 2. providing adequate sunlight to habitable rooms. | <p>No acceptable outcome is nominated.</p> | <p>Complies Buildings have been designed to achieve good solar access. All private open space areas are provided with adequate sunlight.</p> |



| | | |
|---|--|---|
| <p>PO26 Design elements contribute to an interesting and attractive streetscape and building through:</p> <ul style="list-style-type: none"> -the provision of projections and recesses in the façade which reflect changes in internal functions of buildings, including circulation; -variations in material and building form; -modulation in the façade, horizontally or vertically; -articulation of building entrances and openings; -corner treatments to address both street frontages; -elements which assist in wayfinding and legibility; and -elements which relate to the context including surrounding buildings, parks, streets and open spaces. | | <p>Complies</p> <p>The design of the development provides direct streetscape interaction with the internal road network and connection into the wider external road network. Resident access is provided from Pinnacle Drive (connecting missing link between Pinnacle Drive and Sorghum Street) and via private internal road with driveway access to each dwelling. An internal footpath runs along dwelling units 12-22 and 45-48 which links into the external footpath network in Pinnacle Drive/Sorghum Street.</p> <p>The dwelling layouts have been carefully considered, with private spaces situated behind screening, and living spaces have direct access to patio areas. Each unit has a carport and associated tandem visitor car park. Refer to floor plans in Appendix C.</p> <p>Four different colour schemes and materials have been applied to add visual variety.</p> |
| <p>PO27 Roof form assists in reducing the appearance of building bulk by:</p> <ol style="list-style-type: none"> 1. articulating individual dwellings; and 2. incorporating variety in design through use of roof pitch, height, gables and skillions. | <p>No acceptable outcome is nominated.</p> | <p>Complies</p> <p>Four different dwelling types are proposed, refer to Appendix C. The roof forms assist further with the articulation of the entire building experience.</p> |
| <p>PO28 Development promotes the safety of residents and visitors, particularly through casual surveillance of the street and public spaces.</p> | <p>AO28.1 Buildings are designed to have balconies, windows and building openings overlooking streets and other public spaces.</p> <p>AO28.2 Fences or walls along a street frontage have a maximum height of 1.2m where solid, or 1.8m where that portion of the fence above 1.2m high is at least 50% transparent.</p> | <p>Complies</p> <p>The development design allows for passive surveillance to occur within the subject site. Appropriate lighting and signage will be provided throughout the development.</p> <p>Clear sightlines and open layouts in communal areas will be designed to minimise visual obstructions, enhancing natural surveillance and contributing to a secure atmosphere throughout the community. Refer to Appendix D.</p> |
| <p>PO29 Development provides private open space that is:</p> <ol style="list-style-type: none"> 1. well-proportioned, appealing, functional and easily accessible, and promotes outdoor living as an extension of the dwelling; 2. provides a high level of privacy for residents and neighbours; and 3. has sufficient size and shape to meet the needs of a diversity of potential residents. | <p>AO29.1 For a ground floor dwelling (where a multiple dwelling), ground floor private open space is provided with:</p> <ol style="list-style-type: none"> 1. a minimum area of 35m²; 2. a minimum dimension of 3m; and 3. clear of any utilities such as gas, water tanks or air-conditioning units. <p>AO29.2 For a dwelling above ground storey, private open space is provided as a balcony with:</p> | <p>Complies</p> <p>All units are provided a spacious open space areas that are easily accessible from living areas. The open space areas are well in excess of the minimum requirements. Clothes lines are provided within each private open space. All areas of open space are well screened to provide ultimate privacy between units. Refer to Appendix C for detailed dimensions.</p> |



| | | |
|--|--|--|
| | <p>-a minimum area of 9m² for a 1 bedroom unit or 16m² for a 2 or more bedroom unit; -a minimum dimension of 3m; and -clear of any utilities such as air conditioning units or drying space.</p> <p>A029.3 Where clothes drying areas are provided on private balconies they are screened from public view and do not take up more than 10% of the balcony area.</p> <p>A029.4 Private open space is located or screened so it does not directly overlook main living areas or private open space of adjoining dwellings. Where screening is used, it is at least 50% translucent.</p> | |
| <p>PO30 Sufficient communal open space is provided to create flexible spaces suitable for a range of activities.</p> | <p>No acceptable outcome is nominated.</p> | <p>Alternate Solution There will be two communal areas (>500m²) with seating opportunities. The communal space in the southwest corner will include a BBQ. This will provide residents with space to socialise, enhancing community ties and creating informal meeting places. It is not proposed to include additional communal activities within the development site as there are external opportunities within the immediate area. The landscaping concept plans shows connections to nearby parkland. Existing open space areas allow residents to engage in activities such as picnics, casual sports and other recreational activities.</p> |
| <p>PO31 Communal open space provides facilities including seating, landscaping and shade. Treatments such as green roofs, green walls or community gardens contribute to the attractiveness of these spaces.</p> | <p>No acceptable outcome is nominated.</p> | <p>Refer to above.</p> |
| <p>PO32 Air conditioning units are insulated so that adjoining properties are not affected by the noise source, and are not significantly visible from the street.</p> | <p>No acceptable outcome is nominated.</p> | <p>Complies Air condition units are located so that there is no impact on adjoining properties and they are not visible from public viewpoints.</p> |



6.6 DEVELOPMENT CODES

6.6.1 Healthy Waters Code

The purpose of the Healthy Waters Code is to ensure developments manages stormwater and wastewater as part of the integrated total water cycle and in ways that help protect the environmental values specified in the Environmental Protection (Water) Policy 2009.

Response

The Design Flow Technical Memorandum and associated letter of support included in **Appendix J** identify the proposed treatment of stormwater and wastewater in relation to Precinct 4 (Stage 12A) development. The proposed stormwater strategy to manage stormwater quantity, quality and overland flow has been developed in accordance with the Townsville City Plan.

Stormwater Drainage

The proposed stormwater infrastructure shall be designed in accordance with the *Townsville City Plan Schedule 6.4, SC6.4.8 Stormwater Management* and the *Queensland Urban Drainage Manual - Fourth Edition, 2016*.

Minor System

Road pit and pipe drainage will collect minor stormwater flows (2 year ARI, 39% AEP) stormwater which will be discharged to open drains and linear wetlands. The Stage 12A, Lot 907 internal stormwater system will be designed to collect 10-year ARI.

Major System

Major stormwater flows (100 year ARI, 1% AEP) will be contained within the road reserve and will discharge to either an open drain adjacent to South Beck Drive (0.20% gradient), an open drain to the east of Precinct 1 (0.25% gradient) or a linear wetland north of Treeline Drive/Bluewattle Boulevard (flat base, no gradient). Open drains will have a minimum longitudinal grade of 0.2% which is slightly flatter than the minimum grade of 0.3% nominated in the Townsville City Plan.

Stormwater Quantity

The proposed development increases the impervious area when compared with the predevelopment scenario. The interim stormwater quantity strategy is to install an appropriately sized sediment/detention basin to ensure that the post development peak flows are mitigated to predevelopment flow rates. High flow bypasses from the sediment basin will discharge over a spillway and into an open drain.

The ultimate strategy (once 80-90% of Precincts 1 and 2 are built out) is to convert the sediment basin into an end of line wetland. High flow bypasses from the wetland will discharge over a spillway and flow into the existing open drain.

Stormwater Quality Management Plan

A Technical Memorandum has been prepared by Design Flow and is included in **Appendix J**. The stormwater treatment strategy for the development considers an integrated approach for the whole development. The stormwater management strategy focusses on linear wetlands and end of line wetlands. The linear wetlands will facilitate drainage and create green corridors across the subject land.

It is noted that a staged construction approach has been adopted, whereby wetlands are bulked out for construction sediment management prior to completion when approximately 80-90% of the build out phase is complete. This approach offers a high level of protection to downstream waterways from sediment laden runoff during construction.

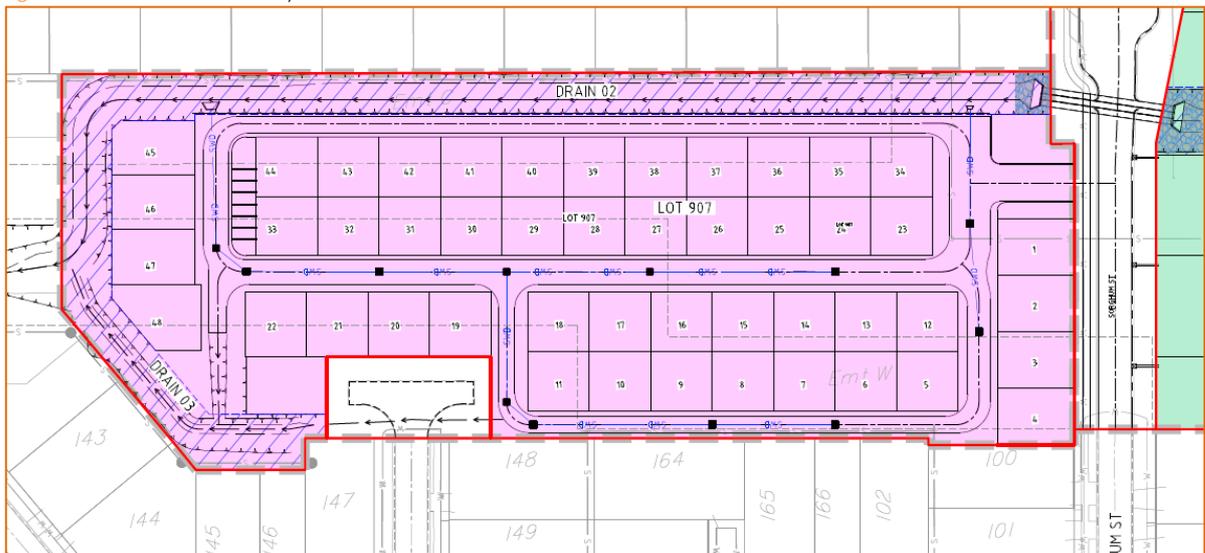


The overall development will proceed from the south to the north, with the southern treatment zone proposed to be implemented first. Construction sediment management proposes to utilise High Efficiency Sediment (HES) basins (Type E), where automated flocculant dosing will be applied to construction runoff to achieve the objectives of the State Planning Policy and Council's policies.

The stormwater strategy is demonstrated in detail within the relevant expert reports and show that compliance can be achieved with the Healthy Waters Code if appropriately conditioned. A detailed assessment against this code is provided in **Appendix E**.

A preliminary stormwater layout plan is shown in *Figure 12 and Appendix E*.

Figure 12: Internal stormwater layout



Source: Empower

6.6.2 Landscape Code

The purpose of the Landscape Code is to ensure landscaping in both the private and public domains is designed and constructed to a high standard, provides a strong contribution to the city image, is responsive to the local character, site and climatic conditions and remains fit for purpose over the long-term.

Response

Extensive and well considered landscaping is proposed as part of the overall development. The overall landscape intent is shown on the concept plans and landscaping concept in **Appendices C and D**. *Figure 13* shows the ground plane strategy with landscaping focal points being:

- Landscape buffer along the southern and eastern boundary;
- Landscaped courtyard;
- Entry statement at the western boundary of the site; and
- Street trees along building frontages.

An overarching park connection plan is demonstrated in *Figure 14*. It shows how the proposed development integrates and connects with the surrounding network.

A detailed assessment against the code has been provided by RPS, refer to **Appendix D**.



Figure 13: Internal landscape layout



Source: RPS

Figure 14: External connection into parkland



Source: RPS



6.6.3 Transport Impact, Access and Parking Code

The purpose of the Transport Impact, Access and Parking Code is to ensure appropriate provision for transport and end of trip facilities, and to facilitate, as far as practicable, an environmentally sustainable transport network.

Response

The proposal is consistent with the purpose and overall outcomes of the Transport Impact, Access and Parking Code.

Access to the development will be from a new driveway crossover onto an extension of Sorghum Street and Pinnacle Drive. The driveway access to Pinnacle Drive has an access width of 7.8m at the property boundary. This is in excess of the minimum access width at property boundary for two-way access for Multiple Dwellings noted on TCC Standard Drawing SD-030. The internal road width is 5.5m. The proposed driveway location provides >100m of sight distance in either direction, complying with AS2890 stopping sight distance requirements for a 50km/h posted speed on the frontage road (69m).

Each dwelling will be provided with one undercover carport and space to park an additional vehicle on the driveway. This parking will be supplemented by six (6) unallocated parking spaces near the western edge of the site. Hence, the proposed total number of car parks for the development is 102 which is well in excess of the requirements (93) stipulated in the planning scheme.

Pedestrian access will be provided by way of a 1.5m wide path along dwelling units 12-22 and 45-48 which links into the external footpath network in Pinnacle Drive/Sorghum Street.

A detailed assessment against the code has been provided by SLR, refer to **Appendix F**.

6.6.4 Works Code

The purpose of the Works Code is to provide with a level of infrastructure which is appropriate to the intended character and function of the site and creating a high expectation for community health, safety and amenity. Development provides suitable access and does not detract from the desired character, amenity, integrity or quality of the locality.

Response

A detailed assessment against the code has been provided by Empower, refer to **Appendix E**.

Water Reticulation

The DMP report in **Appendix I** details the network modelling and infrastructure sizing for the proposed 48 multiple dwelling development in Stage 12A of the Somers & Hervey Precinct 4 area. Further, it illustrates the Upper Ross area and Somers & Hervey Estate is well serviced with a trunk water network.

A preliminary water reticulation layout plan is shown in *Figure 15*.



Figure 15: Internal water reticulation layout



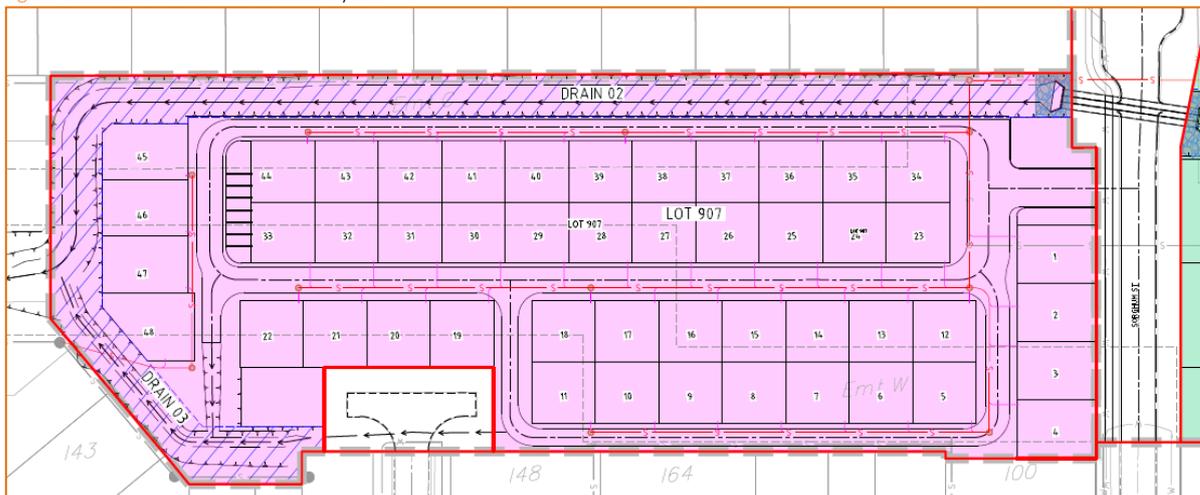
Source: Empower

Sewer Reticulation

The DMP report in **Appendix I** details the network modelling and infrastructure sizing for the proposed 48 multiple dwelling development in Stage 12A of the Somers & Hervey Precinct 4 area.

A preliminary sewer reticulation layout plan is shown in *Figure 16*.

Figure 16: Internal sewer reticulation layout



Source: Empower

Electricity and Telecommunications Services

Design and negotiations by SPA Consulting Engineers are continuing with utility service providers for the supply of electricity and telecommunications to Stage 12A of the Somers & Hervey Residential Estate.

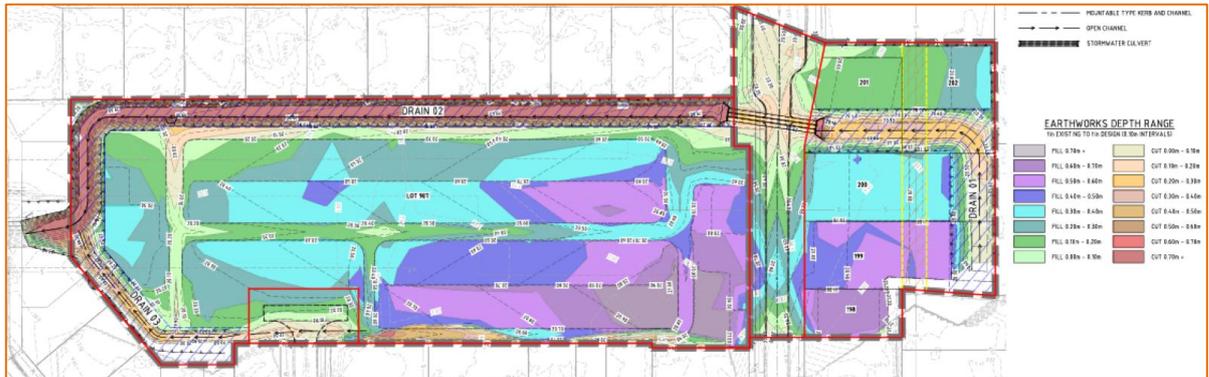
Earthworks

The englobo lot will be filled above Q100 (1% AEP) flood levels, while also allowing for sufficient stormwater/ floodwater conveyance and attenuation through the site.



Further, the englobo lot will be reshaped with a minimum grade of 1 in 200 falling towards the road reserve to alleviate the need for rear of lot roof water drainage systems and the associated easements. Given the very flat nature of the site, operating levels for linear wetlands, end of line wetlands and temporary sediment basins have been carefully considered to allow for a free draining outlet under South Beck Drive. The proposed englobo lot Earthworks Management Plan is shown in *Figure 17* below.

Figure 17: Earthworks management plan



Source: Empower

6.7 OVERLAY CODES

6.7.1 Airport Environs Overlay

The proposed development is nominated for assessment against the Airport Environs Overlay Code.

The purpose of this code is to *ensure development does not negatively impact on the operations of the Townsville Airport, RAAF base or any other aviation facilities. Any development is to ensure that safety is maintained throughout the airport's operational airspace by reducing the effect of aircraft noise on people and minimising risk to public safety near airport runways.*

Response

The subject site is identified in Operational Airspace more than 90m above ground level and located 8km from the airport runway.

The development is considered consistent with the overall outcomes, performance outcomes and acceptable outcomes of the code. In particular, the proposed development anticipates low rise residential housing with no impact on the operational airspace. A detailed assessment against the code is not warranted.

6.7.2 Flood Hazard Overlay

The proposed development is nominated for assessment against the Flood Hazard Overlay Code.

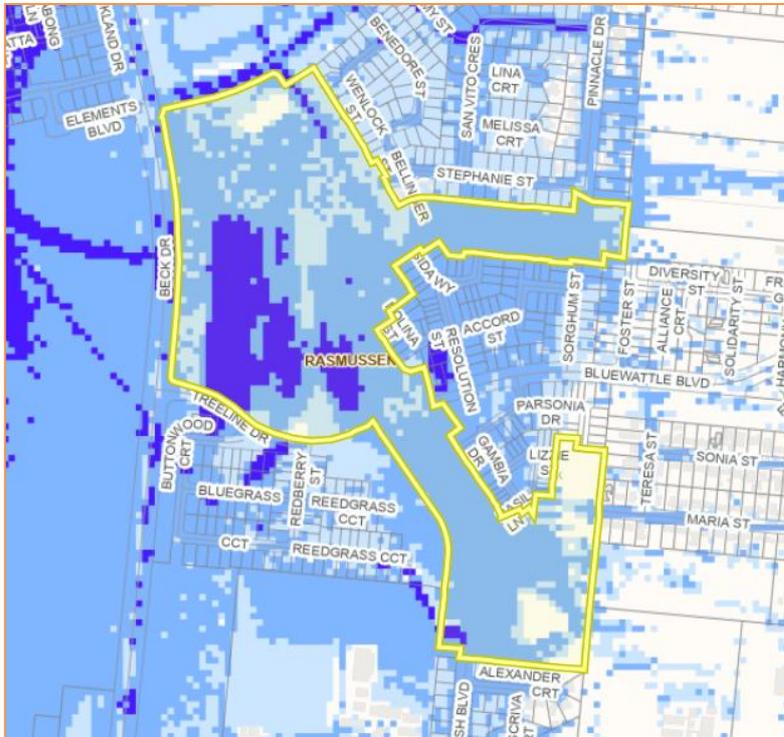
The purpose of this code is to *manage development outcomes in flood hazard areas so that risk to life, property, community, economic activity and the environment during future flood events is minimised, and to ensure that development does not increase the potential for flood damage on-site or to other property.*



Response

The subject site is identified in low, medium and high hazard flood areas, refer to *Figure 18* below.

Figure 18: Council's Flood overlay



Source: TCC Mapping

Flood Impact Assessment

A Flood Impact Assessment to support Precinct 4, Stage 12A has been prepared by Northern Consulting Engineers and are included in **Appendix H**.

The Flood Impact Assessment includes a full assessment against the Flood Hazard Overlay Code – refer to section 6.11 of the Flood Impact Assessment.



7.0 CONCLUSION

This proposal details a development application seeking a Development Permit for a Material Change of Use (Multiple Dwelling) on land at 2 Treeline Drive, Rasmussen, being formally described as Lot 904 on SP344615.

In summary, the proposed development is recommended for approval based on the following reasons:

- The development will contribute to the diversity and choice of housing that is available across the City and will assist in supply at a time of recognised housing pressure. There is a demonstrated demand for affordable housing that is not otherwise currently available in the Townsville market. Provision of this type of housing product will open supply to the market and it provides an alternate product to assist in responding to Council's Townsville Affordable Housing Action Plan which includes in its purpose to support options that seek to improve access to housing, retain and attract key workers as well as activate the city.
- The proposal complies with the codes nominated by the planning scheme as being relevant to the assessment of a proposal of this nature.
- The intended use is in character with the locality, ensuring amenity within the area will be maintained.
- The proposed development is located in an area appropriate for the nature of the use and traffic generated by it.
- Parking is considered to be adequate to meet the demand.
- The potential impacts on amenity have been addressed appropriately.

Given the above facts and circumstances presented in this report, we recommend that Council approve the development subject to reasonable and relevant conditions.