

**NOTES**

**GENERAL**

- It is desirable to provide minimum clearances of 0.5m from property boundary and 0.8m from back of kerb. It is preferred to align the pathway outside the underground services locations situated in the road reserve, working from back of kerb as the first option.
- Kerb ramps to be constructed in accordance with Thuringowa City Council standard drawing 10085.
- Where pathway replaces a residential driveway, upgrade concrete depth to 150mm and place F62 mesh with 25mm top cover. Where driveways are too steep the pathway may need to be diverted towards the property boundary to achieve 2.0% crossfall.
- Where the pathway alignment clashes with the power pole alignment, deviate the pathway around poles to achieve a minimum clearance of approximately 0.2m from the poles.

**JOINTS**

- Provide Tooled joints at a maximum spacing approximately equal to the width of the concrete path. Cut every second bar of mesh under Tooled joint.
- Provide Key Control joint at 6.0m maximum spacing.
- Provide Expansion joint at 24.0m maximum spacing. Joints to be filled with approved filler and sealant.
- Construction joint to be provided adjacent to all concrete property crossings and where any variations occur.

**VEGETATION**

- Grassed area disturbed by construction shall be trimmed and revegetated on completion of the pathway. Where the existing footpath is well grassed and maintained, provide a 0.5m wide strip of turf to both sides of pathway. Turfing to match existing grass species where possible. Where existing footpath is bare or not maintained, provide a 0.5m wide strip of good quality topsoil. Disturbance to established vegetation should be minimised during construction.
- The Contractor shall liaise with the resident and Parks Services prior to removal and trimming of trees. Where trees are removed, the Contractor shall arrange for Parks Services to replace them with a suitable variety of tree outside the pathway alignment.
- The construction site shall conform to the requirements of Environment Management. Refer to the Council Specification "Integrated Environmental Management System (City of Thuringowa) - Construction and Maintenance Environmental Management Plan" (GHD, December 1998).

**SERVICES**

- Existing irrigation pipes and sprinklers disturbed by construction is to be reinstated with sprinkler heads placed 0.5m from the face of kerb.
- Fire Hydrants and Sluice valves disturbed by the construction of the pathway are to be made flush with the pathway and recapped appropriately.
- Electricity, Telecommunications and Gas service covers that are disturbed by the construction of the pathway are to be made flush with the pathway and recapped appropriately. Refer to the relevant Authority for the work to be carried out.

**EXISTING PERMANENT SURVEY MARKS AND IRON PINS**

- The Foreman shall arrange for the City Surveyor to locate and expose the existing PSM's and Iron Pins. Where the PSM and Iron Pin location clashes with the pathway alignment, a cast iron cover box shall be installed and finished flush with the final pathway level as directed by the City Surveyor. The alignment of the pathway may be varied to preserve the PSM's and Iron Pins.

RP743621  
SP801495

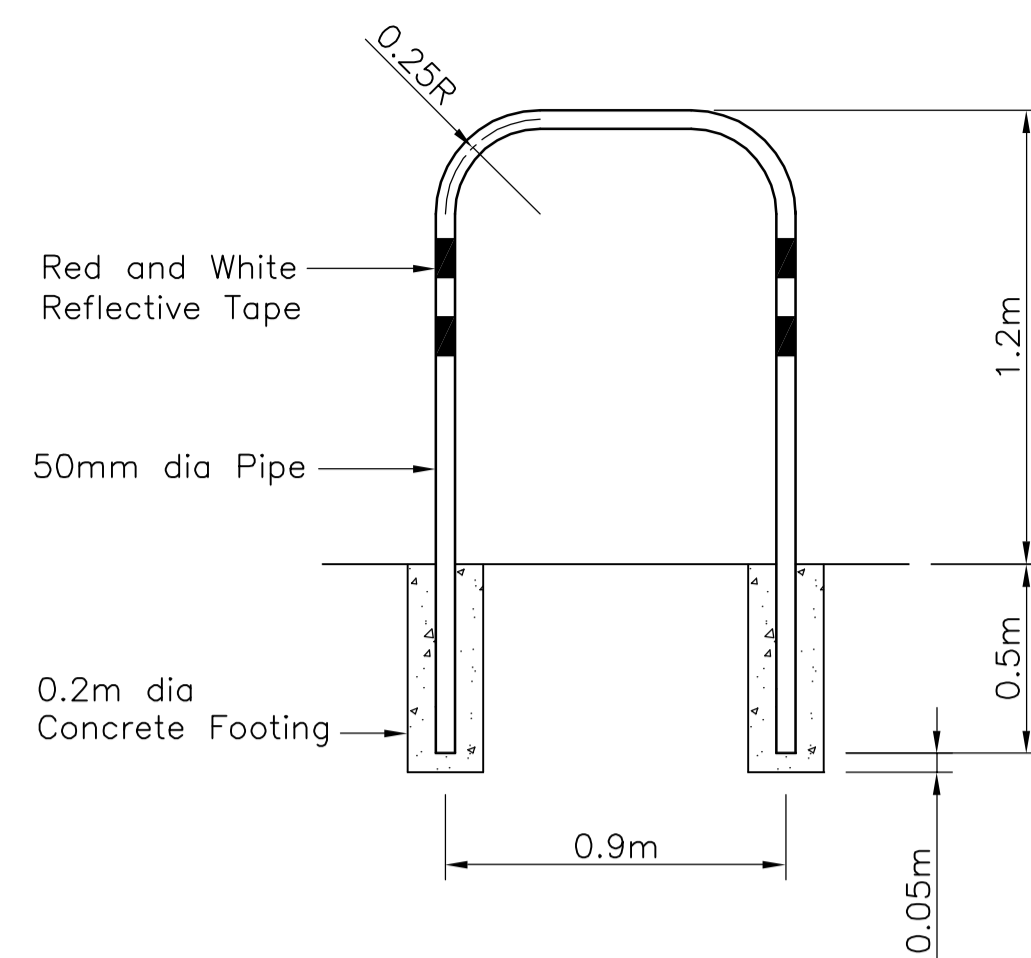
UNDERGROUND SERVICES SHOWN APPROXIMATELY ONLY. EXACT LOCATIONS TO BE DETERMINED ON SITE, BY FOREMAN, PRIOR TO COMMENCEMENT OF CONSTRUCTION.

**LEGEND**

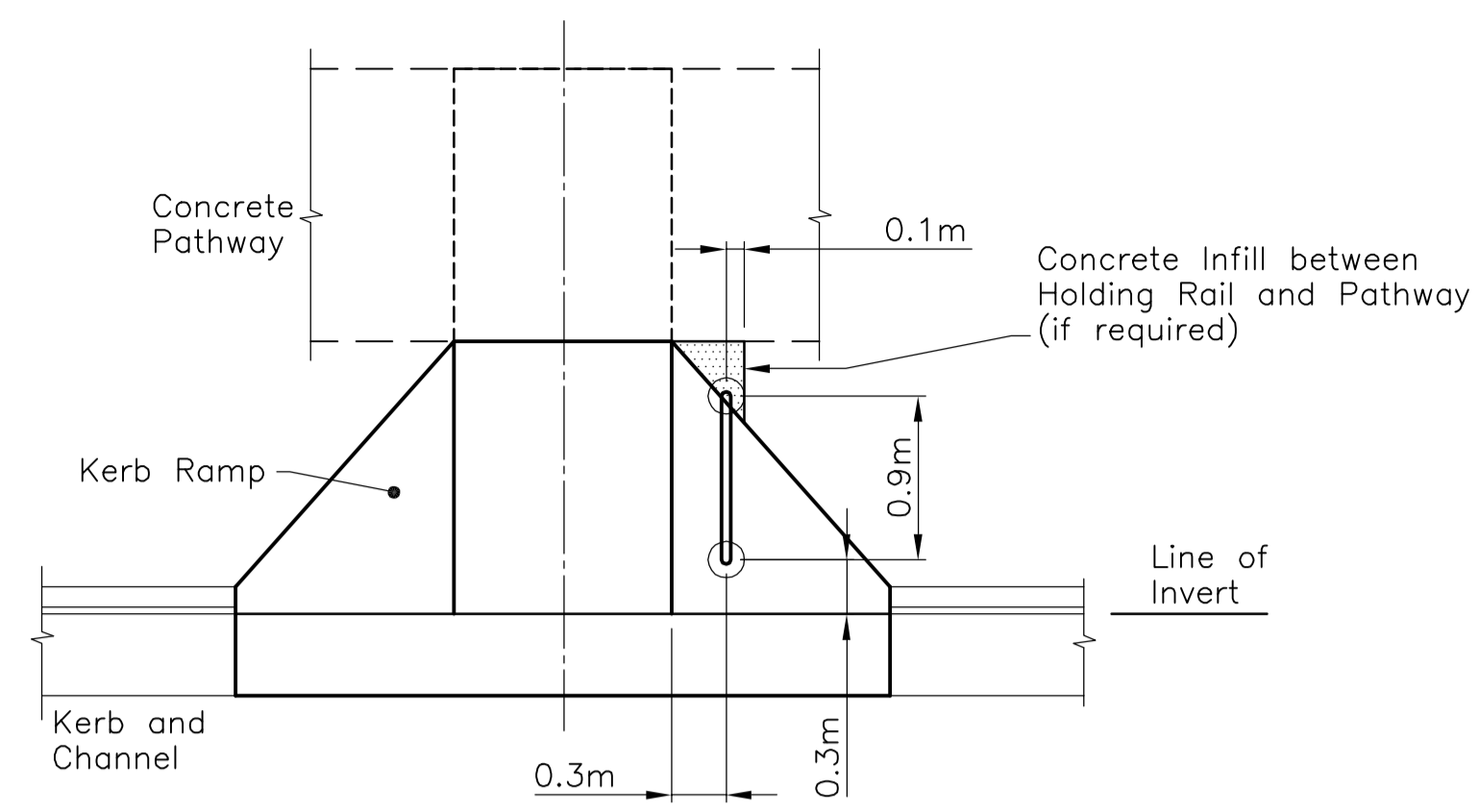
Concrete pathway		Light pole/Power pole	
Asphalt pathway		Telstra pit	
Existing pathway areas		Fire Hydrant/Sluice Valve	
Existing Garden Beds		Iron Pin	
Water main		Permanent Survey Mark	
Stormwater line		Rubbish Bin	
Sewer main/Manhole		Existing Kerb Ramp	
Holding Rail		Stormwater Pit	
Signs			

**STANDARD NOTES AND LEGEND**

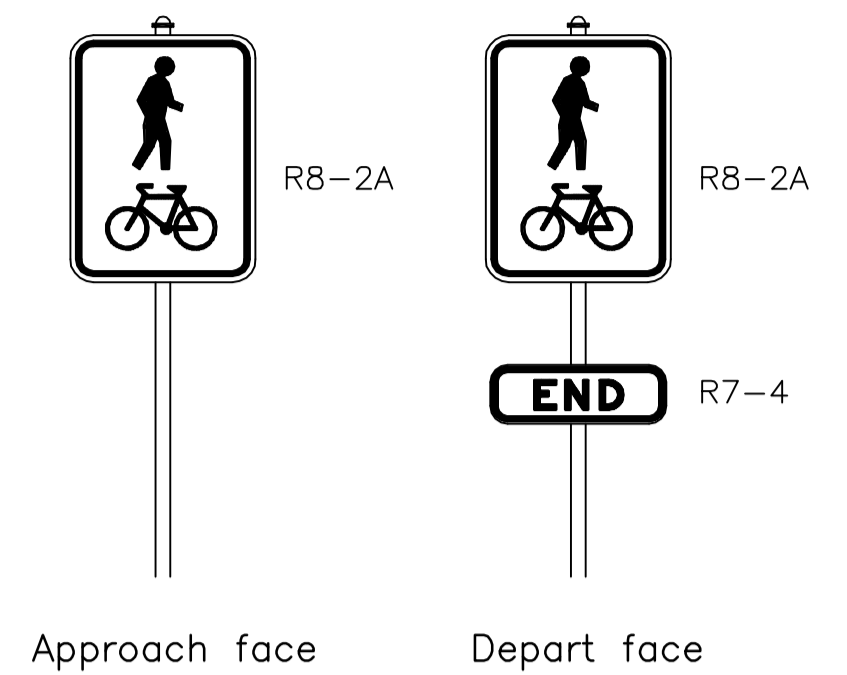
DISPLAYED ON A PATHWAY DRAWING



**HOLDING RAIL DETAIL**

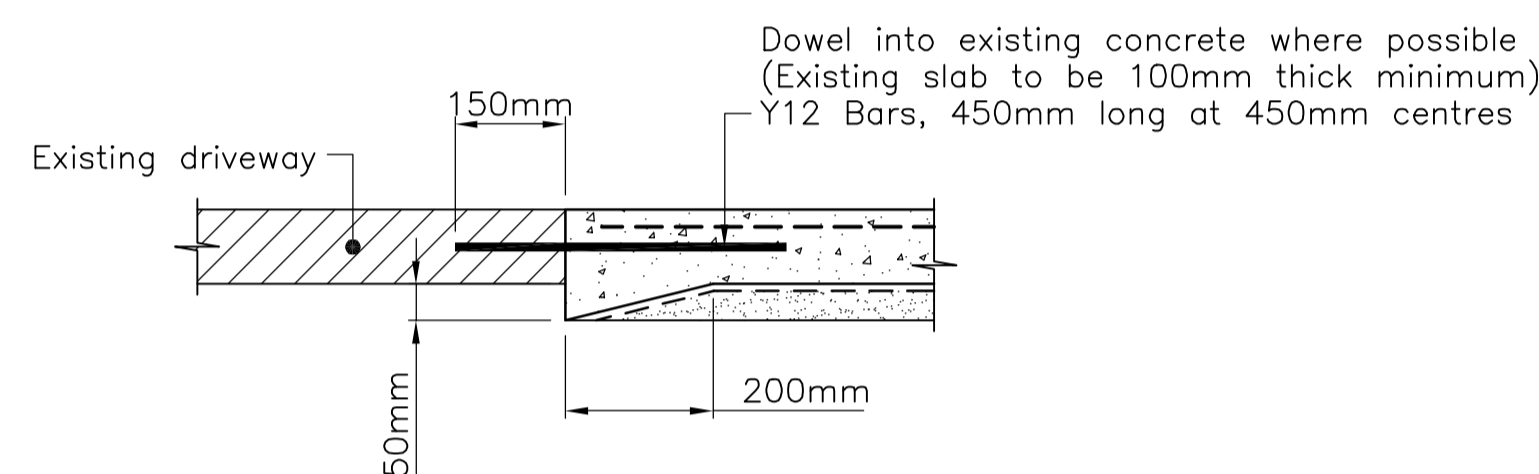


**HOLDING RAIL LOCATION PLAN**

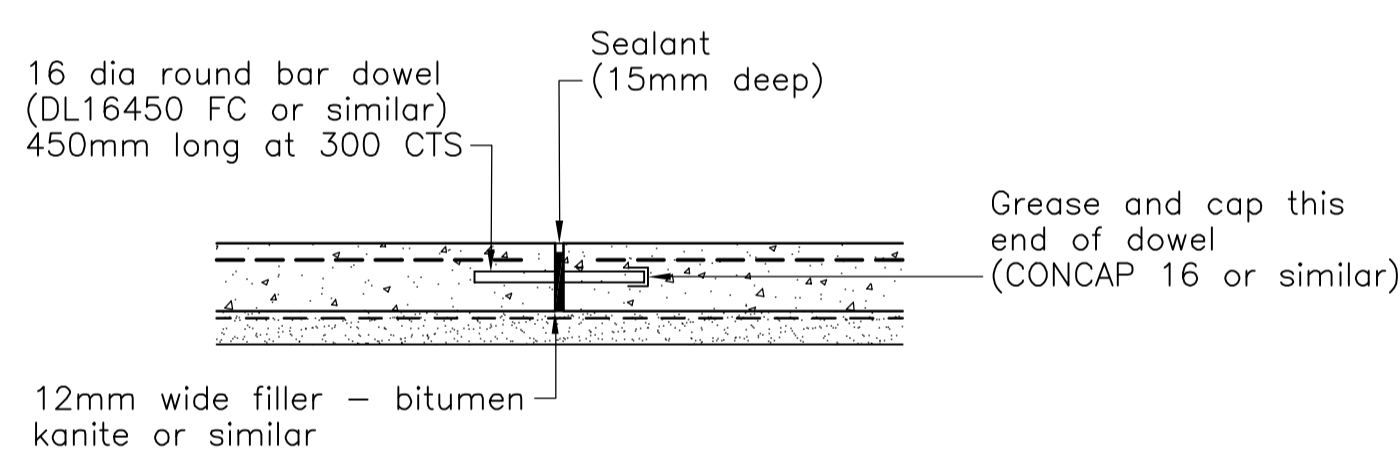


**SIGN DETAIL**

For shared pathways  
2.0m wide or more

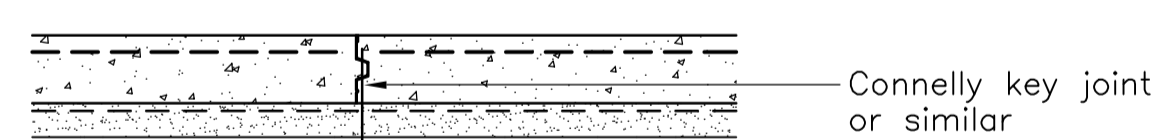


**JOINT AT EXISTING DRIVEWAY DETAIL**



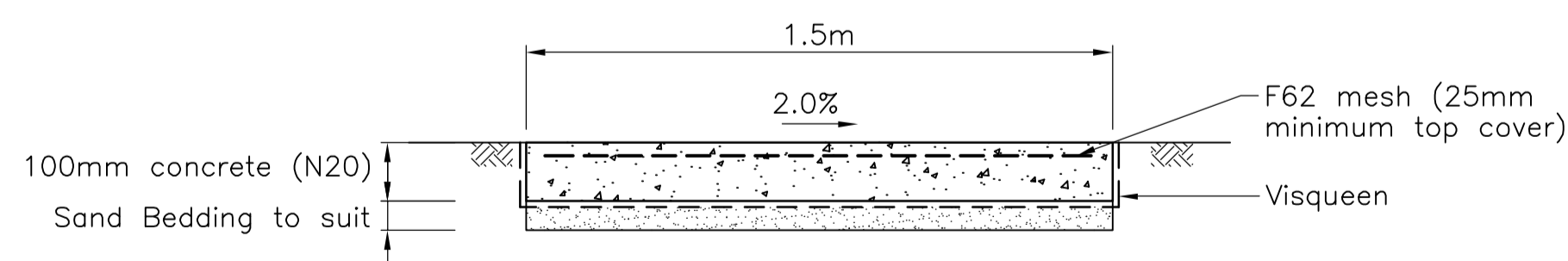
**EXPANSION JOINT DETAIL**

REFER TO NOTE

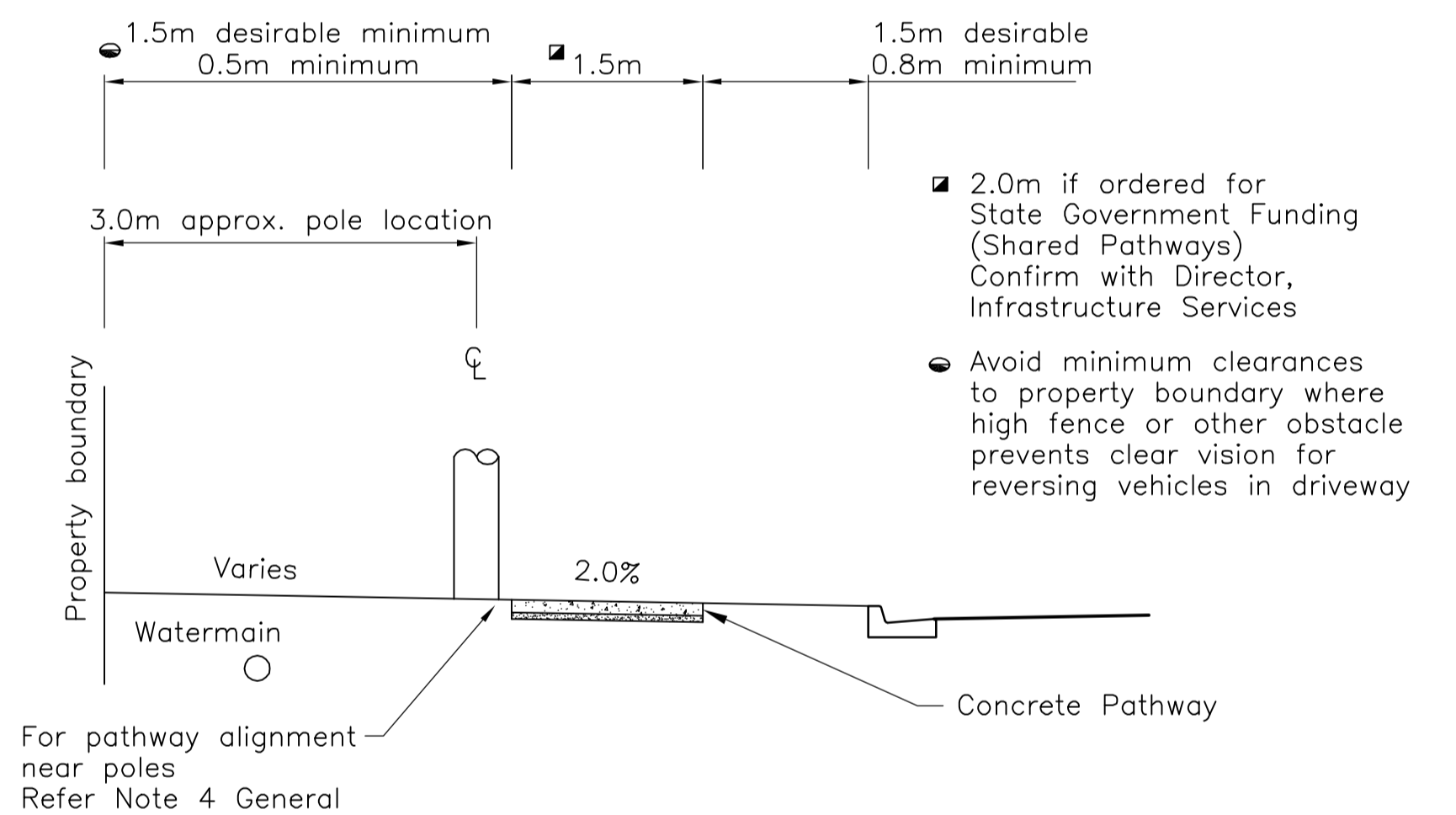


**KEY CONTROL JOINT DETAIL**

REFER TO NOTE



**CONCRETE PATHWAY TYPICAL SECTION**



**1.5m CONCRETE PATHWAY TYPICAL CROSS SECTION DETAILS**

FOR REINFORCED CONCRETE (FIBRE) ALTERNATIVE  
REFER DRAWING 10067

NOTES :  
Not to Scale



THE CITY of  
**THURINGOWA**

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**CONSTRUCTION OF A CONCRETE PATHWAY  
REINFORCED CONCRETE (MESH) ALTERNATIVE**

**STANDARD  
DRAWING  
ROADWORKS**

10066

No.	DATE	DESCRIPTION	AP'D
B	18/10/2004	UPDATE CORSS SECTIONS, ADD HOLD RAIL & CONC PATHWAY TYPICAL SECTION	
A	25/11/2003	ORIGINAL ISSUE	
REVISIONS			

DRAWN: NRM	Engineer Approved: Original Signed By B.Sue	Date: 22/10/04
CHECKED: WJP	Director Approved: Original Signed By B.Bailey	Date: 25/10/04

A	B			
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